

Statistical Analysis of the Questionnaire on the Revision of the IMO Resolution A.857 (20)

CHINA MARITIME SAFETY ADMINISTRATION

VTS users

1229

Captains

Pilots

Agents

Stakeholders



VTS operators

VTS supervisors

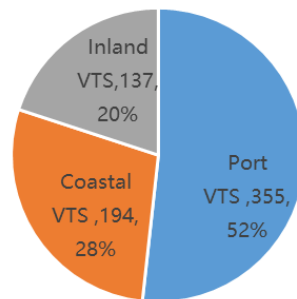
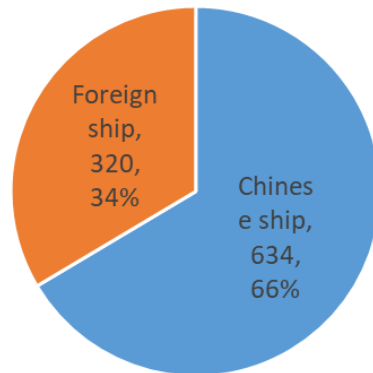
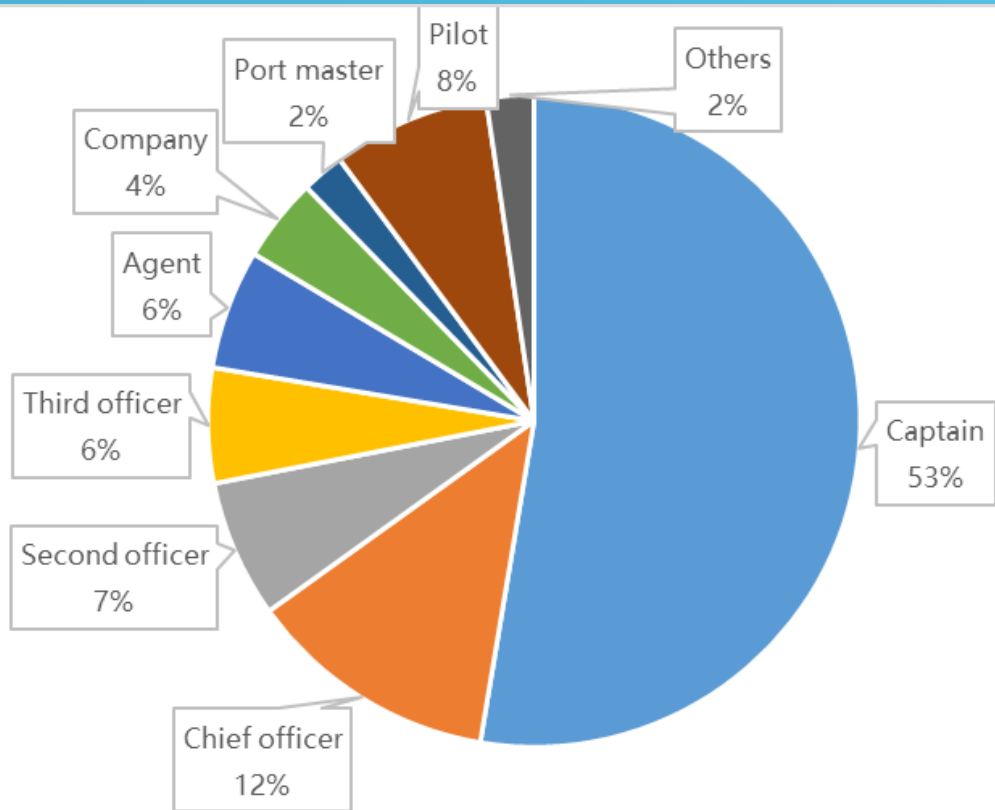
VTS managers

Technical support staffs

VTS personnel

492

Respondents Distribution



MAIN RESULTS

- 1. Some VTS personnel (about 30% of the participants) believed that the definitions of the competent authority(31%), VTS authority(28%), VTS centre(31%), VTS operator(29%), information service(33%), navigation assistance service(29%), and traffic organization service(23%) were unclear or ambiguous.
- 2. More than 80% participants believed that it was necessary to recognize IALA Standards relating to VTS. And more than 70% held the view that it was necessary to update the definition in Annex 2 of the Resolution according to documents such as IALA Recommendation V-103.

MAIN RESULTS

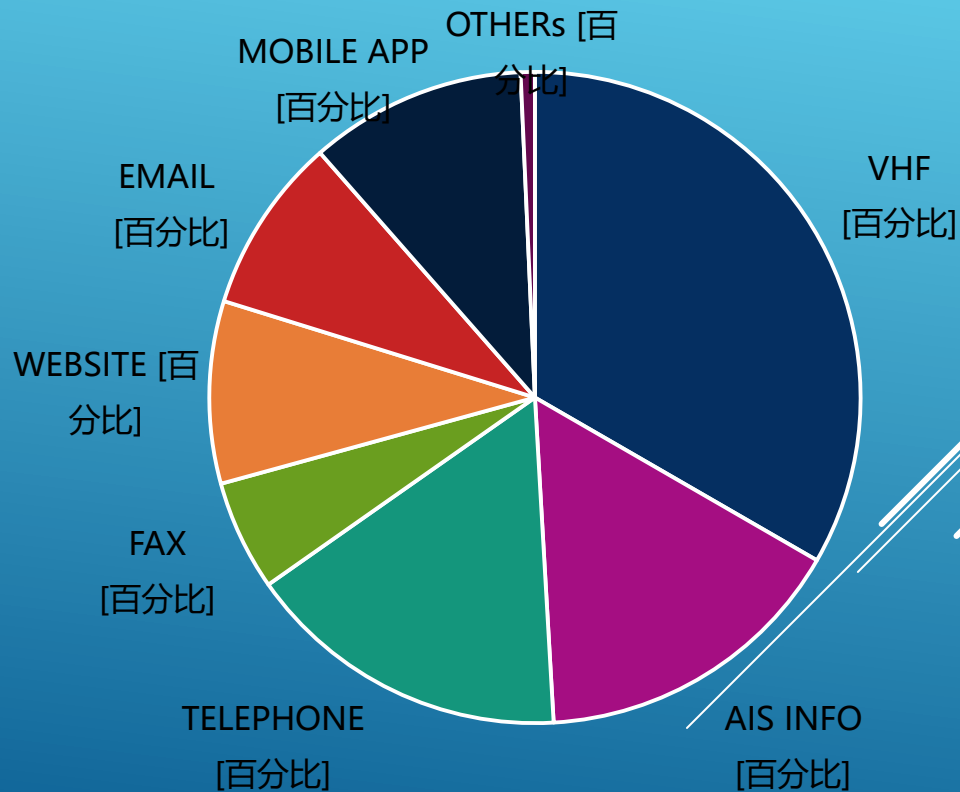
- 3. More than 71% participants thought that it was necessary to clarify the status of VTS beyond territorial seas in the Guideline.
- 4. When the ship is in close-quarters situation or difficult navigational circumstances or in case of defects or deficiencies, 75% participants of VTS operators preferred to provide reminder information instead of instructions or details of execution. However, 69% participants also supported the idea that results-oriented instructions limited the ability of VTS operators to provide effective service to ships.

MAIN RESULTS

- 5. Among the 492 questionnaires collected from VTS staffs, 74% of the respondents thought it was necessary to expand the range of VTS operators. The revised Resolution should cover the different roles of VTS personnel, including VTS manager, VTS supervisor and technical support staff.
- 6. Survey statistics indicates that more than 30% respondents thought the disunities of VTS user's guide would cause confusion and more than 90% believed that it was necessary to establish a unified "VTS user's guide".

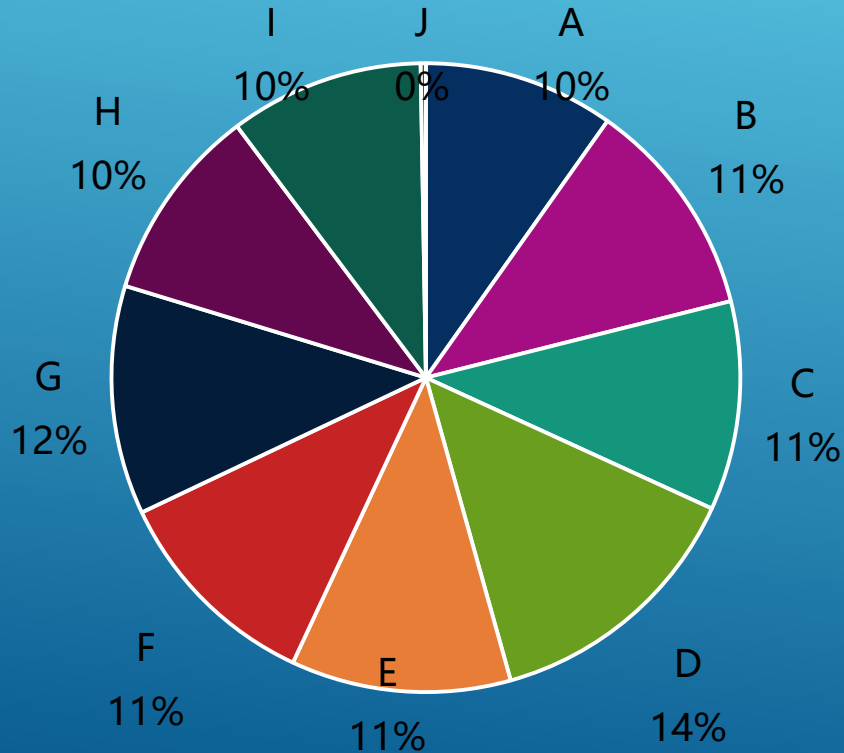
OTHER RESULTS

WAYS TO GET INFORMATION FROM VTS



OTHER RESULTS

INFORMATION NEEDED



- A. Hydro and meteorological information
- B. port schedule information
- C. Anchorage management information
- D. Traffic control information
- E. Obstacle information
- F. AtoN information
- G. Navigational warning
- H. Information to avoid collision
- I. Special rules in VTS area
- J. Others

OTHER ADVICES

- The collision risks between cargo vessels and fishing boats, which do not participate in VTS in China.
- Noisy VHF communication conditions because of busy traffic flow.
The efficiency of information exchange needs improved by expanding the ways and standardizing the language.

Clarification

- Although the questionnaire covered foreign vessels, Chinese ships accounted for a higher proportion. In addition, since the ships were berthing at Chinese ports during the survey, there was no guarantee that the survey results could reflect objectively the general views of global ships to global VTS centers. This is also the case with VTS personnel.

Clarification

- The revision of IMO Resolution A.857 (20) on Guidelines for Vessel Traffic Services should take into account the opinions of both VTS users and VTS personnel, to ensure that the revision is always able to meet the needs of users and conform to VTS work practices.

ADVICE

We advise that,

- consider such survey in subsequent global VTS questionnaire to provide data support for further revision work.
- consider the results of the survey carried out by China MSA, as appropriate.

THANK YOU !

